



FIBER OPTIC OVERBUILD  
PROJECT CHVLVAXA SE  
PERMIT 27000  
AERIAL PERMIT DRAWING  
CITY OF CHARLOTTESVILLE, VA



3				AS-BUILT
2				REVISION # 1
1	3/27/2024	BEC	JRC	ORIGINAL
NO.	DATE	ENG DESIGN	DRAFTING	COMMENT
The logo for Byers Engineering Company, featuring a stylized blue 'B' and the text 'BYERS ENGINEERING COMPANY' in a bold blue sans-serif font.				
PROJECT MANAGER: JAMES COSTNER				
ENGINEERING FIRM: BYERS ENG. CO.				
PROJECT: CHVLVAXA SE				
ADDRESS: CITY OF CHARLOTTESVILLE VA				
PERMIT NUMBER: 27000				
SHEET: C1 OF C3				

# LEGEND & LINETYPES

	AERIAL
	BURIED
	RIGHT OF WAY
	GUARD RAIL
	LOC/CLUSTER BOUNDARY
	RR TRACKS
	EDGE OF PAVEMENT
	GAS
	WATER
	ELECTRICITY
	SEWER
	STORM DRAIN

	GAS TAP/MARKER
	RISER
	TELEPHONE
	POWER VAULT
	GROUND/BOND
	CULVERT
	WING WALL
	SPAN MEASUREMENT
	EXISTING DOWN GUY & ANCHOR
	PROPOSED DOWN GUY & ANCHOR

	UTILITY POLE - EXISTING
	HANDHOLE - PROPOSED
	PROPOSED BORE PIT 4'x14'
	WATER METER
	GAS METER
	FIRE HYDRANT
	VALVE COVER
	AERIAL CROSSOVER

## ABSTEVATION DESCRIPTION


ASW	ASPHALT SIDEWALK
BIP	BLACK IRON PIPE
BOS	BACK OF SIDEWALK
CSW	CONCRETE SIDEWALK
ASPH	ASPHALT DRIVEWAY
CONC	CONCRETE DRIVEWAY
GRVL	GRAVEL DRIVEWAY
GRSS	GRASS DRIVEWAY
EOP	EDGE OF PAVEMENT
EOTW	EDGE OF TRAVEL WAY
FOC	FIBER OPTIC CABLE
HDPE	HIGH DENSITY POLYETHYLENE
HH	HANDHOLE
JB	JUNCTION BOX
MH	MANHOLE
MP	MILE POST
O/S	OFFSET
PVC	POLY VINYL CHLORIDE
RGS	RIDGID GALVANIZED STEEL
RW	RIGHT OF WAY
CL	CENTERLINE



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## GENERAL NOTES / ADDENDUM NOTES

1. THE CONTRACTOR IS REQUIRED TO CONTACT THE APPROPRIATE UTILITY COMPANIES INVOLVED AND MAKE SATISFACTORY ARRANGEMENTS TO ADJUST THE UTILITIES IN CONFLICT WITH THE PROPOSED WORK PRIOR TO BEGINNING CONSTRUCTION.
2. THE CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE, AND LOCAL ENVIRONMENTAL REGULATIONS AND SHALL OBTAIN ALL NECESSARY FEDERAL, STATE, AND LOCAL ENVIRONMENTAL PERMITS, INCLUDING BUT NOT LIMITED TO, THOSE RELATED TO SEDIMENT CONTROL, STORMWATER, WETLAND, STREAMS, ENDANGERED SPECIES, AND HISTORICAL SITES.
3. TRENCHING, BORE PITS, AND/OR OTHER EXCAVATIONS SHALL NOT BE LEFT OPEN OR UNSAFE OVERNIGHT. THE CONTRACTOR SHALL COMPLY WITH ALL OSHA REQUIREMENTS AND PROVIDE A COMPETENT PERSON ON SITE TO SUPERVISE EXCAVATION AT ALL TIMES.
4. ALL FILL AREAS/BACKFILL SHALL BE COMPACTED TO 95% DENSITY IN ACCORDANCE WITH AASHTO T99 AS MODIFIED BY THE NCDOT. ALL MATERIAL TO A DEPTH OF 8 INCHES BELOW THE FINISHED SURFACE OF THE SUBGRADE SHALL BE COMPACTED TO DENSITY EQUAL TO AT LEAST 100% OF THAT OBTAINED BY COMPACTING A SAMPLE OF THE MATERIAL IN ACCORDANCE WITH AASHTO T99.
5. VEGETATIVE COVER SHALL BE ESTABLISHED ON ALL DISTURBED AREAS IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE DIVISION ROADSIDE ENVIRONMENTAL ENGINEER.
6. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE N.C.D.O.T. STANDARDS AND SPECIFICATIONS MANUALS.
7. ANY DRAINAGE STRUCTURE DISTURBED OR DAMAGED SHALL BE RESTORED TO ITS ORIGINAL CONDITION AS DIRECTED BY THE DISTRICT ENGINEER.
8. ALL DRIVEWAYS ALTERED DURING CONSTRUCTION SHALL BE RETURNED TO A STATE COMPARABLE WITH THE CONDITION OF THE DRIVEWAYS PRIOR TO CONSTRUCTION.
9. RIGHT OF WAY MONUMENTS DISTURBED DURING CONSTRUCTION SHALL BE REFERENCED BY A REGISTERED LAND SURVEYOR AND RESET AFTER CONSTRUCTION.
10. PROPER TRAFFIC CONTROL DEVICES, SIGNS, ETC., SHALL BE INSTALLED TO ENSURE PUBLIC SAFETY IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND ANY SUPPLEMENTS THERETO AND NCDOT ROADWAY STANDARD DRAWINGS.
11. ALL LANES OF TRAFFIC ARE TO BE OPEN DURING THE HOURS OF 6:00AM TO 9:00AM AND FROM 4:00PM TO 7:00PM MONDAY THRU FRIDAY. TWO WAY TRAFFIC WILL BE MAINTAINED AT ALL TIMES.
12. INGRESS AND EGRESS SHALL BE MAINTAINED TO ALL BUSINESSES AND DWELLINGS AFFECTED BY THE PROJECT.
13. ANY WORK REQUIRING EQUIPMENT OR PERSONNEL WITHIN 5' OF THE EDGE OF ANY TRAVEL LANE OF AN UNDIVIDED FACILITY AND WITHIN 10' OF THE EDGE OF ANY TRAVEL LANE OF A DIVIDED FACILITY SHALL REQUIRE A LANE CLOSURE WITH APPROPRIATE TAPERS.
14. NO PARKING OR MATERIAL STORAGE SHALL BE ALLOWED ALONG THE SHOULDERS OF ANY ROADWAY.
15. DURING NON-WORKING HOURS, EQUIPMENT SHALL BE REMOVED FROM THE RIGHT OF WAY.
16. ALL ROADWAY SIGNS THAT ARE REMOVED DUE TO CONSTRUCTION SHALL BE REINSTALLED AS SOON AS POSSIBLE.
17. EXCAVATION MATERIAL SHALL NOT BE PLACED ON THE PAVEMENT. DRAINAGE STRUCTURES SHALL NOT BE BLOCKED WITH EXCAVATION MATERIALS.
18. ALL MANHOLES, SPLICE BOXES AND OTHER APPURTENANCES WITHIN THE NCDOT RIGHT OF WAY SHALL BE LOCATED AT/OUTSIDE THE RIGHT OF WAY LINE. MANHOLES AND/OR VAULTS SHALL NOT BE PLACED IN THE DITCH LINE, SIDE SLOPES OF THE DITCHES OR IN THE PAVEMENT.
19. ALL MANHOLES, SPLICE BOXES, AND/OR VAULTS WITHIN NCDOT RIGHT OF WAY SHALL BE OF A PRE-APPROVED DESIGN.
20. PROPOSED TRAFFIC-BEARING MANHOLES AND VALVE COVERS SHALL BE FLUSH MOUNTED AND SHALL BE OF A NCDOT APPROVED DESIGN FOR HS-20 LOADING.
21. ALL WORK TO BE PERFORMED IN STRICT ACCORDANCE WITH THE APPLICABLE CODES OR REQUIREMENTS OF ANY REGULATING GOVERNMENTAL AGENCY, DUKE ENERGY, AND THE RIGHT-OF-WAY GRANTOR.
22. LOCATIONS OF SOME OF THE PHYSICAL FEATURES WERE OBTAINED FROM DATED EVALUATION MAPS OR OTHER DRAWINGS AND MAY NOT SHOWN OR DEPICTED ON THESE DRAWINGS.
23. UNDERGROUND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE RECORDS AND FIELD OBSERVATIONS BUT ARE NOT NECESSARILY EXACT. THEREFORE, UTILITY LOCATIONS WILL BE VERIFIED AT LEAST 100 FEET IN ADVANCE OF TRENCHING OR FLOWING, SO THAT CHANGES IN CABLE PLACEMENT CAN BE MADE IN THE EVENT OF CONFLICT.
24. ALL KNOWN BURIED OBSTRUCTIONS ARE SHOWN ON THE CONSTRUCTION DRAWINGS. ANY AND ALL OTHERS ENCOUNTERED ARE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE, PROTECT, AND REPAIR IF DAMAGED.
25. ANY AND ALL IMPROVEMENTS, SUCH AS ASPHALT OR CONCRETE PAVEMENT, CURBS, CUTTERS, WALKS, DRAINAGE DITCHES, EMBANKMENTS, SHRUBS, TREES, GRASS SOD, ETC., IF DAMAGED, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND RESTORED TO ORIGINAL CONDITION.
26. EQUIPMENT TYPES SPECIFIED HEREIN (IE: "BACKHOE," "SWAMP PLOW" ETC.) ARE SUGGESTIONS ONLY AND ARE NOT INTENDED AS REQUIREMENTS. CONTRACTOR WILL BE NOTIFIED AS TO EXPECTATIONS.
27. SHORING OF BORE PITS AND TRENCHES IN ACCORDANCE WITH OSHA REGULATIONS SHALL BE MANDATORY.
28. BURIED CABLE MARKERS WILL BE PLACED AT ALL UNDERGROUND UTILITY LOCATIONS AND ALL OTHER LOCATIONS IN ACCORDANCE WITH THE PERMIT DRAWINGS AND THE OUTSIDE PLANT HANDBOOK.
29. ALL CONDUIT WILL BE SDR 11 OR AS SPECIFIED.
30. UNDERGROUND CONDUIT WILL BE PLACED AT 36" MINIMUM COVER UNLESS SPECIFIED ON THE CONSTRUCTION DRAWINGS.

## ADDENDUM NOTES

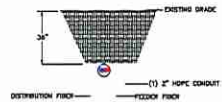
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## CONSTRUCTION DETAILS

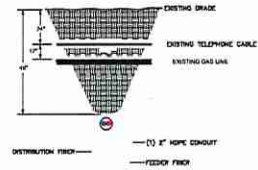
**TYPICAL DETAIL "A"**  
CROSS SECTION OF PHOTOCOLD HOPE



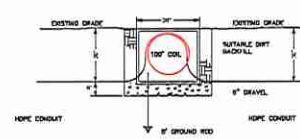
TYPICAL DETAIL "B"  
DIRECTIONAL BORE CROSS SECTION  
FOR CONDUIT



**TYPICAL DETAIL "C"**  
**DIRECTIONAL BORE CROSS SECTION**  
**FOR CONDUIT PLACED BENEATH / PERPENDICULAR**  
**TO EXISTING UTILITIES:**

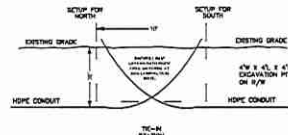


**TYPICAL DETAIL "D"**  
**HANDHOLE CONSTRUCTION DETAILS**  
**CONDUIT TO HANDHOLE PROFILE**



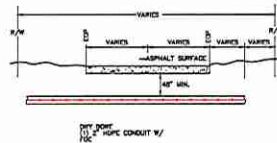
\* ALL EXCAVATIONS ON TRENCHES 4 FEET OR GREATER IN DEPTH SHALL BE APPROPRIATELY BROWDED, SHORED OR SLOPED ACCORDING TO THE PRECIPITATES AND REQUIREMENTS SET FORTH IN OSHA'S EXCAVATION STANDARD, 29 CFR 1926.850, .851, and .852.

**TYPICAL DETAIL "E"**  
DIRECTIONAL ROOF TIP-IN DETAIL

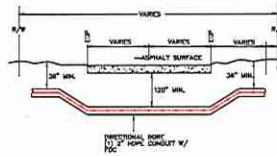


- \* BORE FROM EACH DIRECTION IS RUN AT DESIGN DEPTH TO BORE, FLOW TEST THE TIE-IN, THEN TURNED OFF TO SATURATE.
- \* THE TIE-IN POINT IS EXCAVATED, PIPES CUT OFF WHERE THEY CROSS EACH OTHER AT DESIGN DEPTH, AND COUPLER IS INSTALLED TO CONNECT THE TWO PIPES AT THE DESIGN DEPTH.
- \* USE ALLURION HYDRAULIC PUMP - ON BARRED RING COUPLER TO CONNECT PIPE.
- \* ALL EXCAVATIONS ON TROCHES A FEET OF GREATER DEPTH SHALL BE APPROPRIATELY BEHIND, SHORED, OR SHELDED ACCORDING TO THE PROCEDURES AND REQUIREMENTS SET FORTH IN OSHA'S EXCAVATION STANDARDS, 29 CFR 1926.850, 851, AND 852.

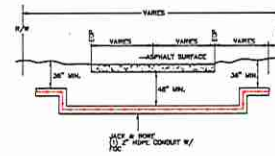
TYPICAL DETAIL "F"  
CROSSING SECONDARY ROADWAY  
AND DRIVEWAY



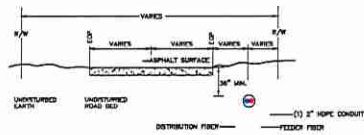
TYPICAL DETAIL "C"



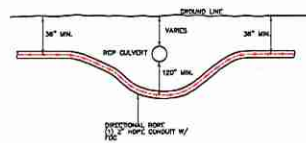
**TYPICAL DETAIL "H"**  
**CROSSING PRIMARY ROADWAY**




TYPICAL DETAIL "I"  
PARALLEL CONDUIT DETAIL  
FOR NCDOT RIGHT OF WAY



TYPICAL DETAIL "J"  
CULVERT CROSSING DETAIL



Know what's below.  
Call before you dig.

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SHEET: CS OF CS				



## BONDING AND GROUNDING DETAILS

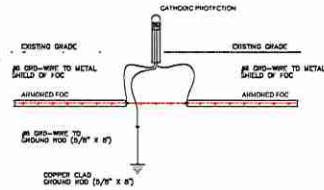
### AERIAL NOTES:

1. Establish and maintain continuity of all metallic components (strength member, shield, moisture barrier, armor) across all aerial splices.
2. Bond metallic components to the support strand at all splice locations.
3. Bond support strand to pole MOHV at all riser poles, fiber loop (2,000') locations for future splice and fiber splice locations.
4. Place bonds between all metallic cable components and the support strand at least once every 1 1/4 miles (0,000 feet).

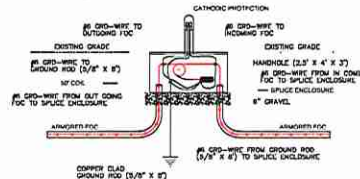
### BURIED NOTES:

1. Establish and maintain continuity of all metallic sheath components and strength members in the cable and across all buried splices.
2. Bond metallic sheath components and strength members to 5/8" x 8' copper clad ground rod at all buried splices.
3. Place 5/8" x 8' copper clad ground rods at all handholes for future splicing and grounding.
4. Place bonds between all metallic cable components and copper clad ground rods at least once every 1 1/4 miles (0,000 feet).
5. Place Cathodic Protection Test Station at all buried fiber cable splices and when bonding fiber metallic cable components to copper clad ground rods every 1 1/4 miles.

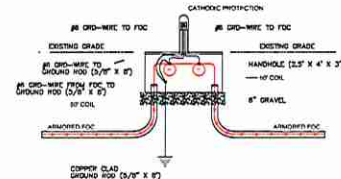
TEST STATION DETAIL SCHEMATIC



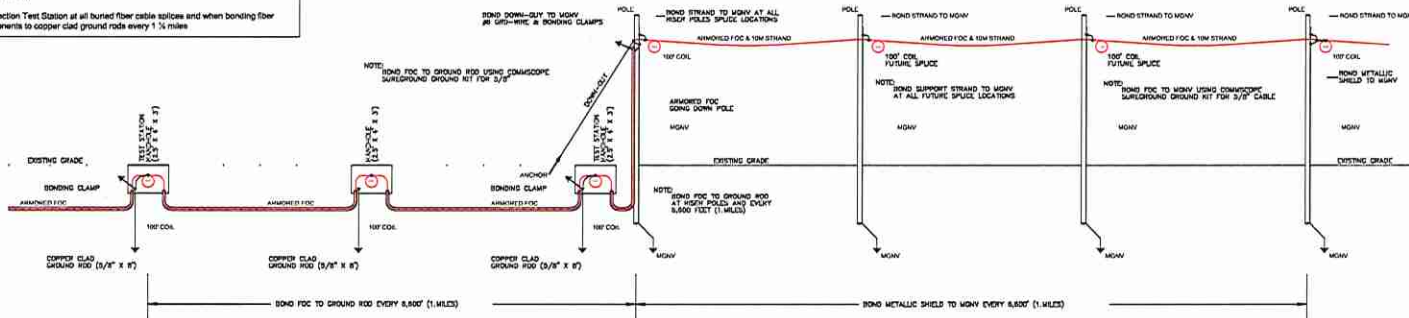
TEST STATION WITH FIELD SPUCE



TEST STATION WITH COIL FOR FUTURE SPUCE



LAYOUT DETAIL




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# VICINITY MAP



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# BOM

Unit Description	UOM	ESTIMATED QTY	ACTUAL QTY
FEEDER CABLE - 24 CT	FOOT		
FEEDER CABLE - 48 CT	FOOT		
FEEDER CABLE - 72 CT	FOOT	1588	
FEEDER CABLE - 96 CT	FOOT		
FEEDER CABLE - 144 CT	FOOT		
FEEDER CABLE - 288 CT	FOOT	2383	

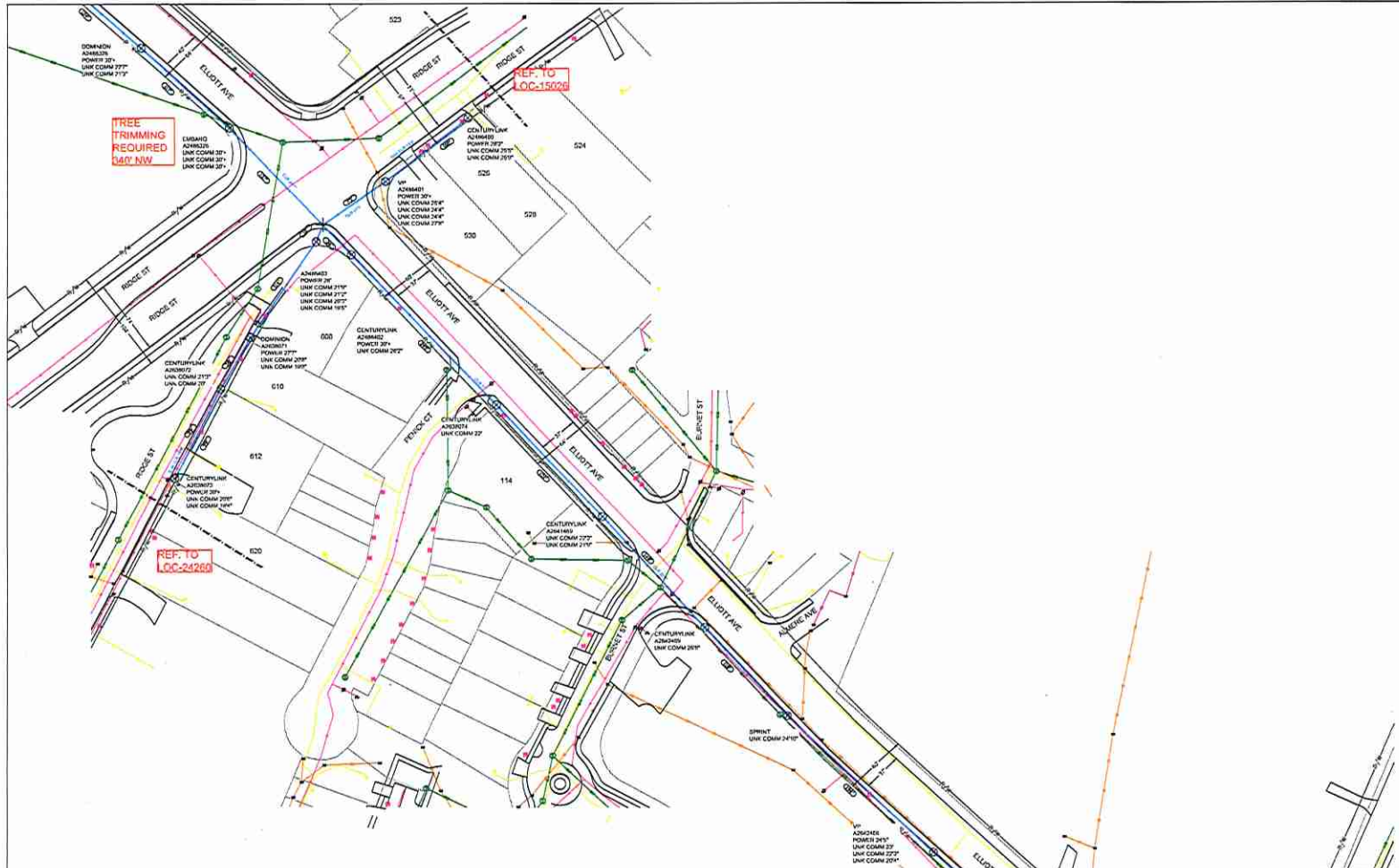
Unit Description	UOM	ESTIMATED QTY	ACTUAL QTY
(1) 2" HDPE INNERDUCT	FOOT	198	
24"DX24"WX36"L HANDHOLE	EACH		
18"DX17"WX30"L HANDHOLE	EACH		

Unit Description	UOM	ESTIMATED QTY	ACTUAL QTY
PLACE DTAP - AERIAL	EACH		
PLACE DTAP - IN HANDHOLE	EACH		
PLACE OPTICAL TAP/EVOLVE - AERIAL	EACH		
PLACE OPTICAL TAP/EVOLVE - IN HANDHOLE	EACH		
AERIAL PRIMARY 10FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 75FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 150FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 300FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 500FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 800FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 1000FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 1500FT PUSHLOCK DROP	EACH		
AERIAL PRIMARY 2000FT PUSHLOCK DROP	EACH		
PULLING PRIMARY JUMPER 10FT	EACH		
PULLING PRIMARY JUMPER 75FT	EACH		
PULLING PRIMARY JUMPER 150FT	EACH		
PULLING PRIMARY JUMPER 300FT	EACH		
PULLING PRIMARY JUMPER 500FT	EACH		
PULLING PRIMARY JUMPER 800FT	EACH		
PULLING PRIMARY JUMPER 1000FT	EACH		
PULLING PRIMARY JUMPER 1500FT	EACH		
PULLING PRIMARY JUMPER 2000FT	EACH		

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MATCH TO SHEET 03



MATCH TO SHEET 02

CONTACT CITY ARBORIST  
STEVE GAINS  
434-970-3260  
BEFORE TRIMMING

ALL PROPOSED AERIAL  
IS TO BE OVERLASHED  
TO EXISTING STRAND

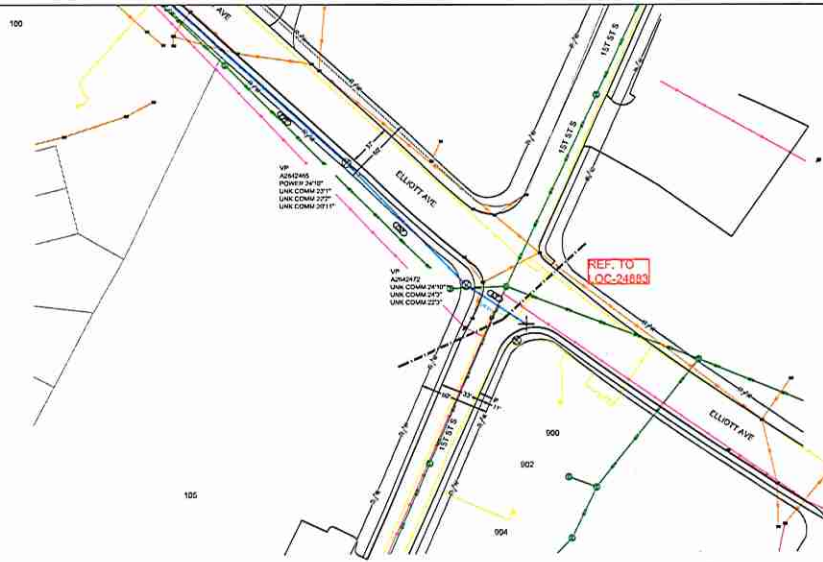
ALL UTILITY PLACEMENTS ARE FOR REFERENCE

SCALE IS FOR REFERENCE PURPOSES ONLY. THE CABLE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE APPROPRIATE PERMITTING MUNICIPALITY PRIOR TO BEGINNING CONSTRUCTION, IN ORDER TO VERIFY THAT THE CABLE IS BEING PLACED WITHIN THEIR RIGHT-OF-WAY.



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MATCH TO SHEET 01




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434-970-3260  
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MATCH TO SHEET 04

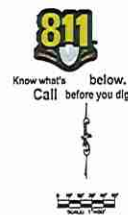
MATCH TO SHEET 01

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STEVE GAINS  
434-970-3260  
BEFORE TRIMMING

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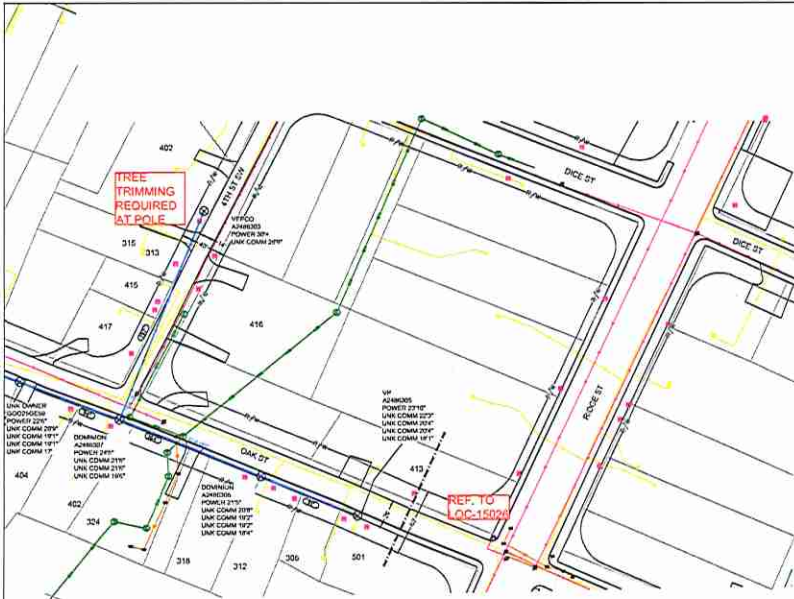
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